

# TAGNPPS 95 / 101 / 103 - IFT TAT

## OPERATING MANUAL



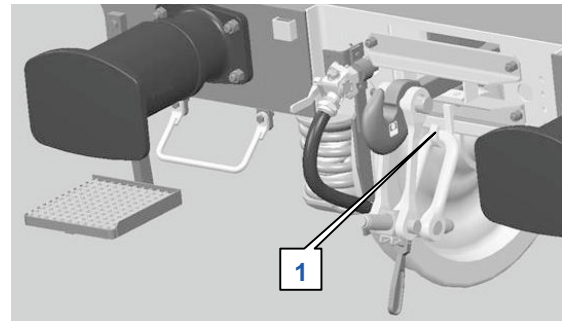
4-axle wagon Tagnpps  
is a covered discharging wagon designed  
for transportation of grain.

Wagon equipped  
with transition platform,  
external ladder and footboards  
in the tank interior.



**CAUTION**

- During climbing and descending the wagon and during entering the vessel and getting off, the operating staff has to take care of their own safety and shall be very careful in order to avoid injuries caused by:
  - Slipping on the footboards, ladders or platforms (mainly in winter)
  - Tripping or hitting over firm wagon parts or other obstacles located in the vicinity of the climbing elements
- The operating staff can descent from the wagon only when they face the wagon (same as during climbing on the wagon), and they have to use footboards and handles. When using the ladder, it is necessary to visually check correct climbing or descending of the ladder!
- In order to avoid injuries, unused screw coupling has to be hanged on a coupling hook **(1)** (Fig. 1).



**Fig. 1**

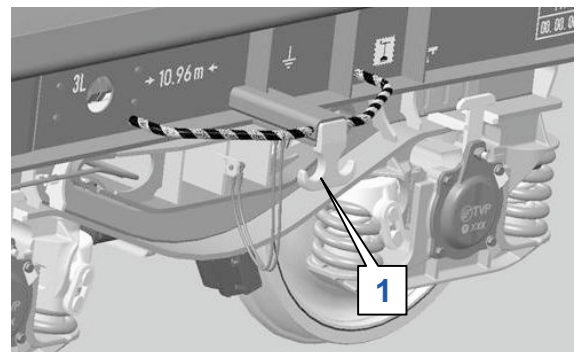
**SIDE TOWING OF WAGON**

For this purpose, the wagon is on both sides equipped with hooks for towing rope **(1)** in accordance with applicable TSI WAG and UIC regulations **(Fig. 2)**.



**CAUTION**

**In order to move the wagon, it is allowed to use only those design parts (e.g. drawing hook, hook for towing rope) that were created for that purpose. Drawing of the wagon via buffers and other parts which were not created for that purpose is forbidden.**



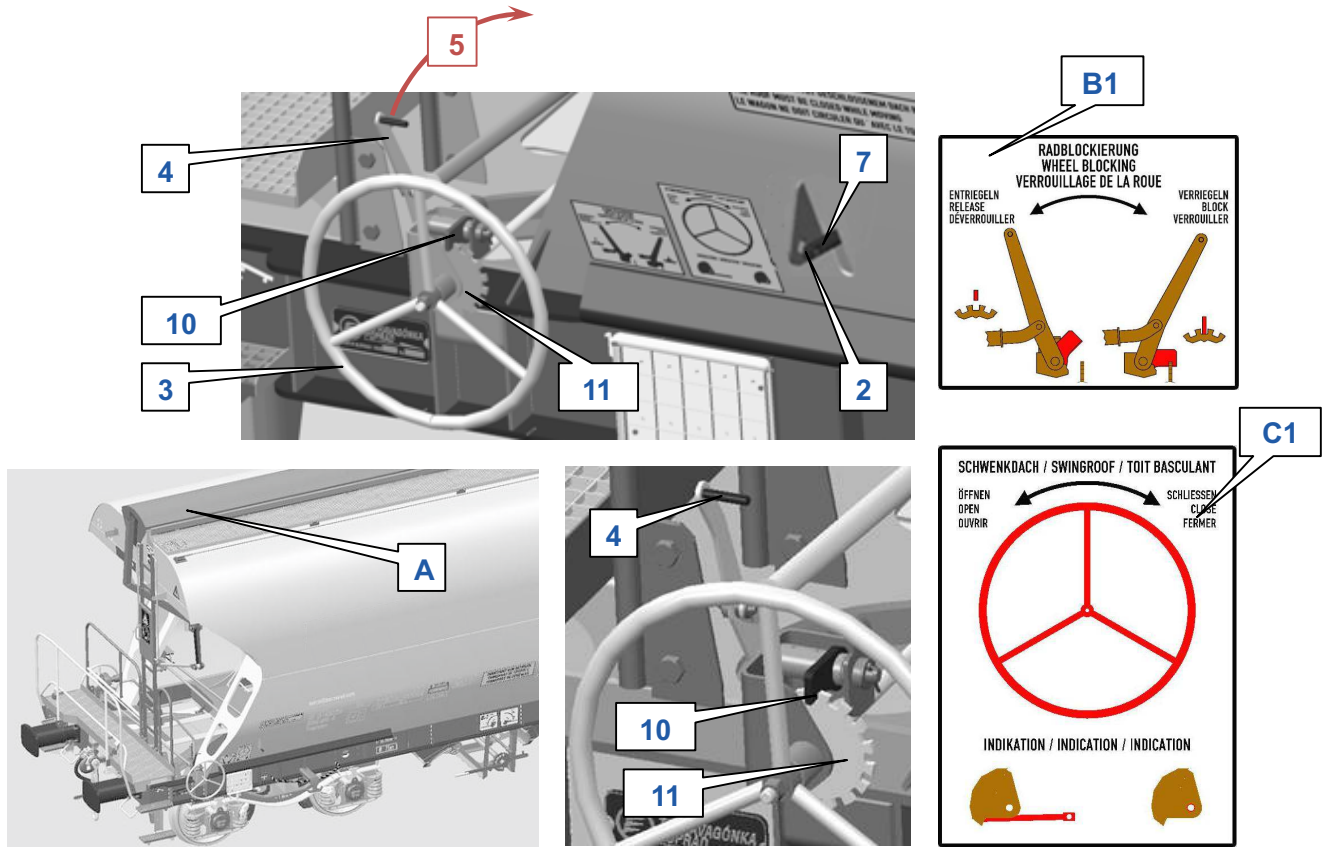
**Fig. 2**

## FILLING OF THE VESSEL

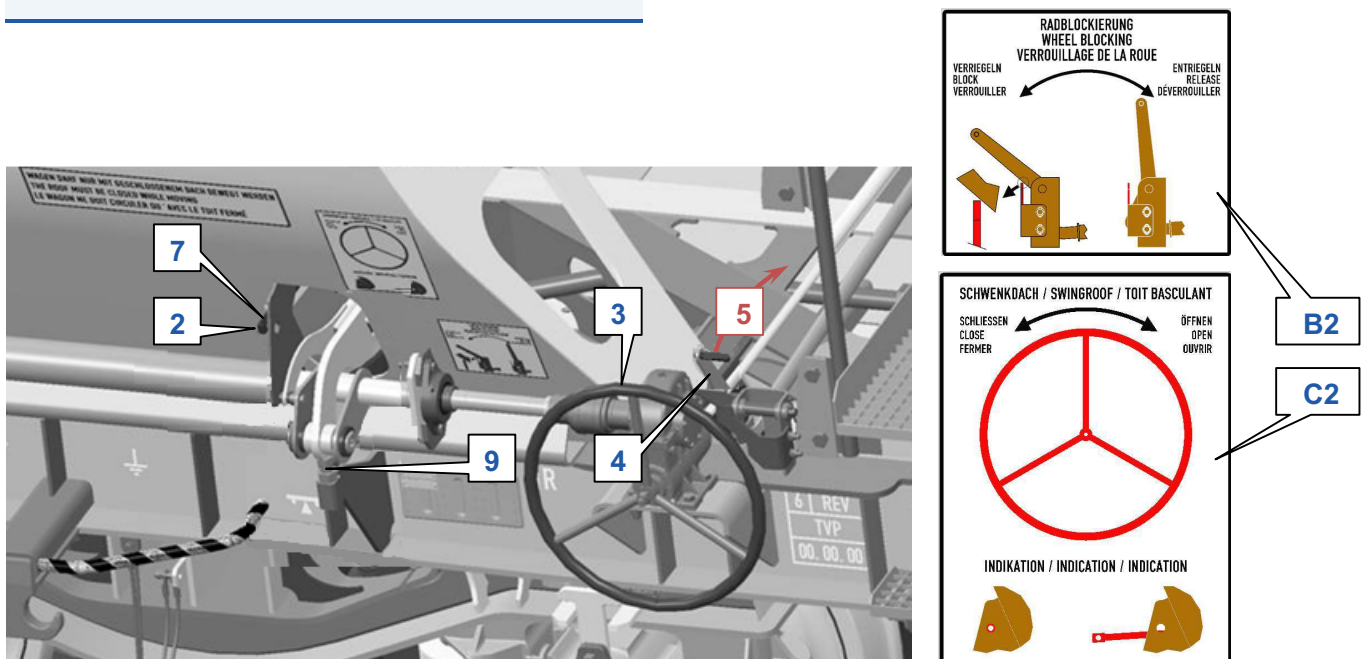
- Before loading, check technical condition of the wagon and the vessel.
- Remove the seal from the area (2) hindering manipulation with the wheel (3).  
**Warning:** Seals are located on both wagon sides.
- **ATTENTION! It is necessary to remove especially metal-wire seals.**  
**If they are left there, the mechanism will be damaged!**
- If any shortcomings are discovered, remove them professionally and inform the wagon keeper.
- Before filling, check if all three hoppers are closed and if control mechanism of each hopper is sealed in the areas (1).
- Unlock the hand wheel (3) by pushing the latch (4) lever in the direction of the arrow (5), i. e. according to the corresponding pictogram (B1+B2).
- Open the roof (A) by turning the hand wheel (3) in the direction of the arrow „OPEN“ - according to the corresponding pictogram (C1+C2).
- Visually check if the roof is opened:
  - Arm of the roof (A) leans against the stops on the fronts (6) (Fig. 5),
  - indicator (7) is in the position according to (Fig. 5).
- Fill the vessel.
- Close the roof (A) by turning the hand wheel (3) in the direction of the arrow „CLOSE“ to the end position - according to the corresponding pictogram.
- Visually check if the roof is closed:
  - Indicator (7) has to be in the position according to (Fig. 6),
  - Arm (8) sits on the stop (9) (Fig. 6).
- Position of the hand wheel (3) shall be secured by closing the latch (4), i.e. the latch tooth (10) shall be inserted into the opening in the ratchet (11).
- On both wagon sides, secure position of the wheel (3) by seals at the sealing areas (2).
- Before moving the wagon, remove securing means (wheel stops, wedges) and release the hand brake (if the wagon is equipped with the hand brake).

**The wagon can be operated only with closed roof!**

**Fig. 3 - left longitudinal side of the wagon**

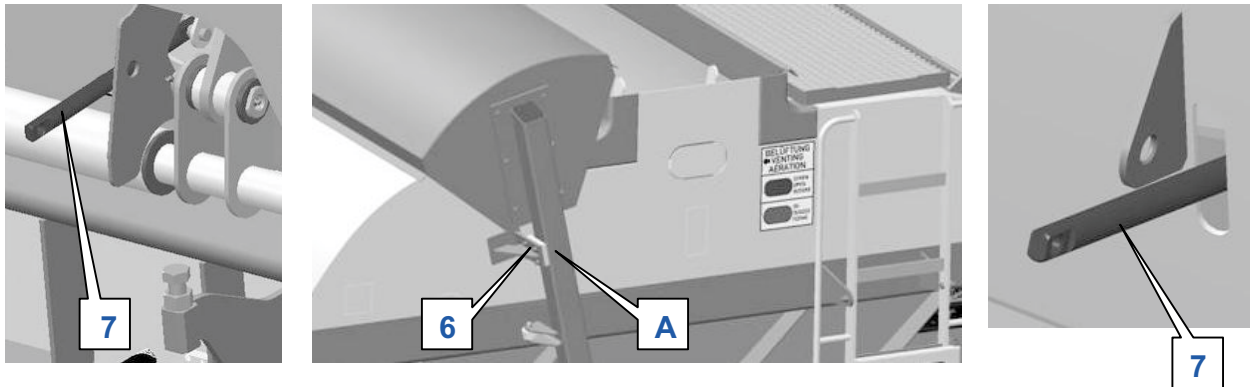


**Fig. 4 - right longitudinal side of the wagon**

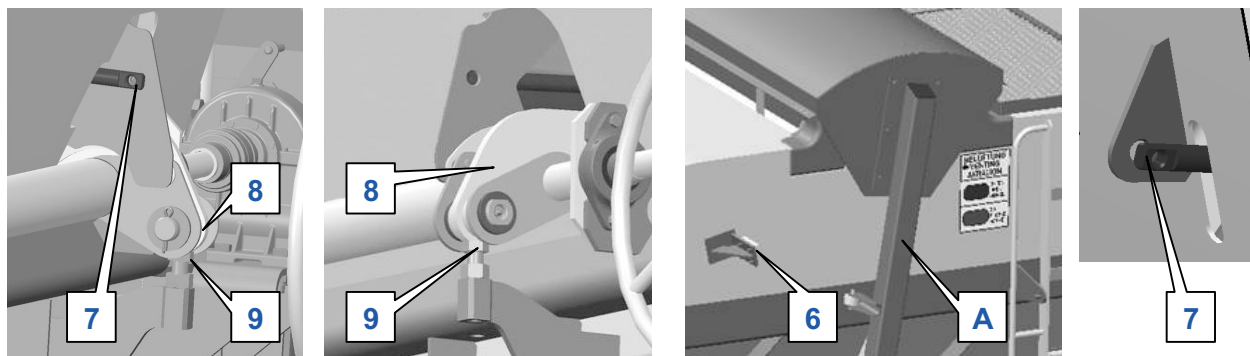




**Fig. 5 - position of the indicator (7) for opened roof**



**Fig. 6 - position of the indicator (7) for closed roof**



## DISCHARGING OF THE VESSEL

- Check technical condition of the wagon and the vessel.
- Before start of discharging, the control mechanism of each hopper shall be unsealed in the area (1).
- **Warning:** Seals are located on both wagon sides. Turning direction of levers controlling the hoppers is depicted by the pictograms (D1) or (D2) above the control levers.
- **ATTENTION! It is necessary to remove especially metal-wire seals. If they are left there, the mechanism will be damaged!**
- If any shortcomings are discovered, remove them professionally and inform the wagon keeper.
- Grab the handle (22) of the latch lever (21) and turn it by ca 180°, by that, the latch nose (23) will release from the groove (24) and then the latch tooth (25) will get to the ratchet tothing (26). Then, grab the handle (27) and gradually turn, whereby, if necessary the latch tooth (25) can block the position of the ratchet (26) position in any position in the tothing. It is possible to turn the handle (27) by approximately 180°. By turning the handle (27), the discharging device will open.
- Discharge the vessel.
- Closing of the hopper: Grab the handle (22) of the latch lever (21) and turn it back by ca 180°, by that, the latch nose (23) will get to curved surface (28). Then, grab the handle (27) and turn it back by ca 180°. By turning the handle (27), the curved surface (28), is turned; the latch nose (23) will fit to the groove (24). After closing of the hopper, it is possible to seal it in the area (1).
- Repeat the process for all three hoppers.
- Before moving the wagon, remove securing means (wheel stops, wedges) and release the hand brake (if the wagon is equipped with the hand brake).

Fig. 7 - left longitudinal side of the wagon

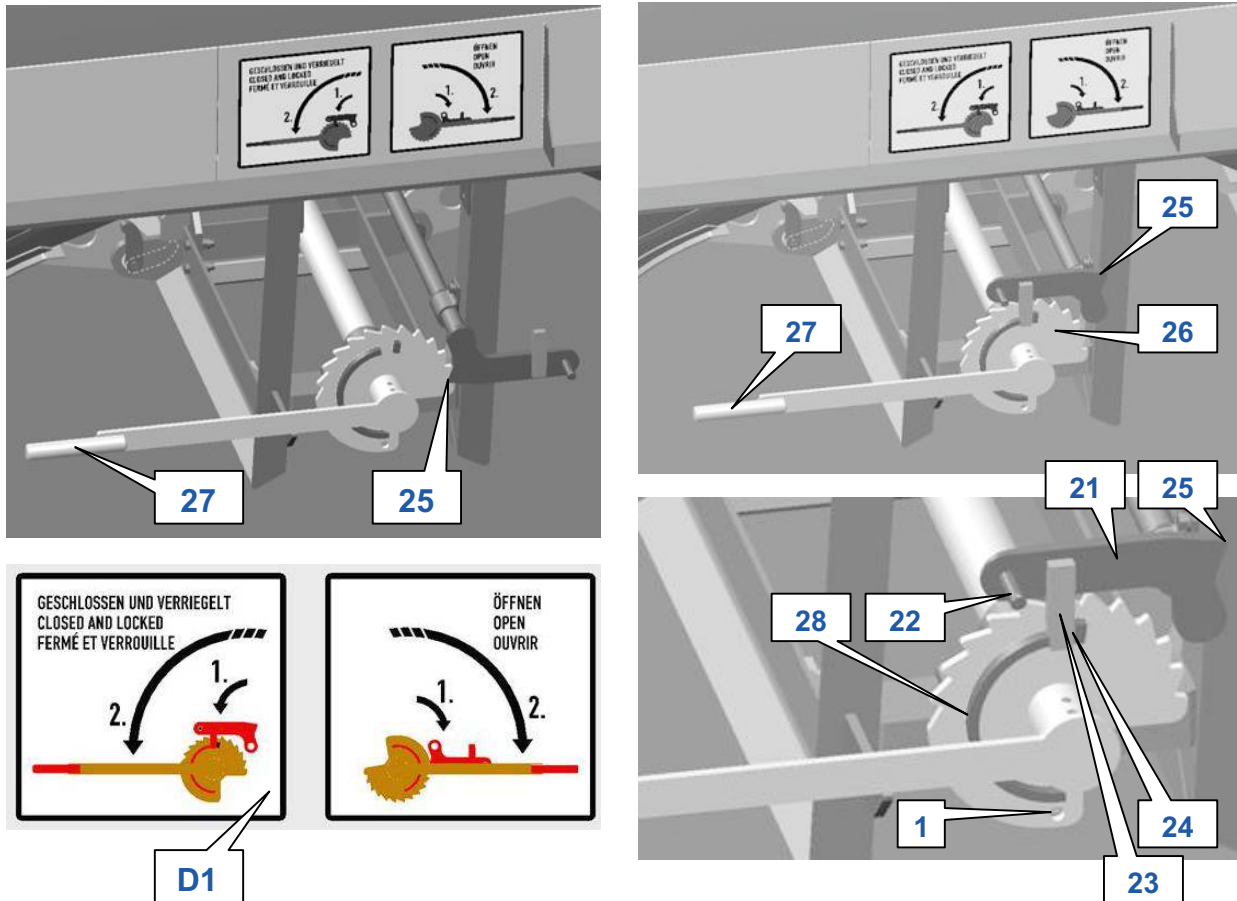


Fig. 8 - right longitudinal side of the wagon

